

# Winnipeg Police Service



## Photo Enforcement Program

2022 Annual Report



**JUSTSLOWDOWN**

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# Winnipeg Police Service

# Photo Enforcement Program

ANNUAL REPORT 2022

## EXECUTIVE SUMMARY

The City of Winnipeg Photo Enforcement Program was established in 2003 to augment conventional enforcement for the purpose of enhancing traffic safety. This program has shown itself to be operationally efficient and effective. This is the 19<sup>th</sup> annual report highlighting the operation of this program.

### HIGHLIGHTS FOR 2022

The contract with Conduent Business Services Canada Inc. (Conduent) was set to expire in May 2022. The City of Winnipeg and Conduent signed a one-year extension for the continuation of the Photo Enforcement Program.

The number of Intersection Safety Camera (ISC) locations remained at 49. A total of 33 cameras were rotated through these locations. In 2022, a total of 10,604 red light offences and 27,321 speeding offences were captured at ISC locations (total 37,925). This represents an increase of 4.45% in ISC violations from 2021 (total 36,309).

The Winnipeg Police Service continued its mobile enforcement and deterrent efforts within school, playground and construction zones throughout the year.

Mobile Photo Radar (PR) captured 72,940 vehicles in violation. This represents a decrease of 19.76% in violations from 2021 (total 37,925).

The Winnipeg Police Service continued to work in collaboration with Argyle (formerly Changemakers) on the successful "Just Slow Down" campaign. Argyle was able to bring the educational program to a broader audience through a variety of communication and marketing platforms.

## 1. REASON FOR THE REPORT

The 2022 Annual Report on the Photo Enforcement Program is submitted to the Province of Manitoba under the Conditions of Authority agreement between the City of Winnipeg and the Province of Manitoba regarding Image Capturing Enforcement Systems.

This report presents the status and effectiveness of photo enforcement by providing an historical overview of the program in Winnipeg and mandatory statistical data as outlined in the agreement. Historical reports are available on request and last year's report is accessible on the [Winnipeg Police Service website](#).

## 2. PROGRAM GOAL

The goal of the Photo Enforcement Program is to decrease speed related fatalities, collisions and injuries. Photo Enforcement encourages drivers to reduce their speed and stop red light running.

## 3. HISTORY OF PHOTO ENFORCEMENT IN WINNIPEG

On October 19, 2001, the Winnipeg Police Service began a six-month pilot project of photo enforcement technology. A single ISC was installed at Sherbrook Street and Broadway. This intersection was chosen due to its comparatively high rate of collisions versus the traffic volumes.

[\*The Highway Traffic Amendment and Summary Convictions Amendment Act\*](#) was passed into legislation on May 22, 2002, and came into force on January 1, 2003, which provided authority for the use of a Photo Enforcement Program in Manitoba.

In November 2002, a two-month warning phase of the Photo Enforcement Program at the city's 12 ISC locations began. This warning phase concluded on January 6, 2003 with over 15,000 violations noted.

On January 7, 2003, the Winnipeg Police Service began issuing tickets for speeding and red-light offences at the 12 ISC locations that were tested during the fall of 2002. Five mobile PR units also began enforcement in school, playground and construction zones.

Twelve new ISC locations were added to the program in August 2003. An additional 12 ISC locations were added in 2004 and again in 2005, bringing the total to 48 locations with 30 cameras being rotated through these sites.

In March 2006, the photo enforcement contract was re-negotiated to bring the total number of mobile PR units from 5 to 10. The number of ISC locations remained at 48 as the re-negotiation removed the original requirement for installation of an additional twelve locations.

As a result of the effectiveness of this road safety initiative, both statistically and by way of public acceptance, the City of Winnipeg committed to continuing the program for a further 60 months. After a lengthy Request for Proposal (RFP) and evaluation process in 2007, the new photo enforcement contract was awarded to ACS Public Sector Solutions.

In 2009, the City of Winnipeg Police Service contracted with the Traffic Injury Research Foundation to conduct an evaluation of the City of Winnipeg Photo Enforcement Program. As a result of this evaluation, four additional ISC locations were established, with two locations being moved late in 2009, followed by two new installations in early 2010. This report is available through the [Traffic Injury Research Foundation](#).

In 2012, the Province of Manitoba introduced amendments to the *Image Capturing Enforcement Regulation* allowing for the use of digital technology.

November 2012 marked the expiration of the city's previous Photo Enforcement contract. As such, the City of Winnipeg issued an RFP for qualifying companies that would see the Photo Enforcement Program move to digital technology.

On December 31, 2012, following the completion of this process, the City of Winnipeg and ACS Public Sector Solutions Inc. (a Xerox Company) signed a contract for the continuation of the Photo Enforcement Program through May of 2021.

In January 2013, the Photo Enforcement Program commenced the equipment upgrade to digital technology. This upgrade was completed in the fall of 2013. The number of ISC locations was increased to 50 with 33 cameras being rotated through the sites. Laser-based technology for mobile PR units was introduced which allowed for PR units to deploy in areas which were not previously conducive to vehicle mounted radar deployments.

In October 2014, the City of Winnipeg introduced legislation creating reduced speed school zones. In these zones, speed limits on streets adjacent to certain school properties was reduced to 30kph Monday through Friday, from 7:00 am to 5:30 pm from September to June. The deployment models for mobile PR units was adjusted at this time to accommodate the change in speed limits at different times during the day.

In January 2017, Xerox began operating as Conduent Business Services Canada. In November 2017, a web portal was introduced. This allows persons receiving a photo enforcement ticket to view high resolution colour images of their violation before their initial response dates.

In 2019, the Province of Manitoba issued an RFP for qualified companies to undertake a review of the Photo Enforcement Program. The review was postponed as a result of the coronavirus pandemic and the review has been delayed indefinitely.

In May 2020, the seven-year contract with Conduent was set to expire. The City of Winnipeg and Conduent signed a one-year extension for the continuation of the Photo Enforcement Program in 2020 with a second one-year extension signed in 2021, and a third one-year extension signed in May 2022.

The Winnipeg Police Service is committed to continuing the efforts to modify driver behavior through the use of automated enforcement technology.

## 4. CHALLENGES AND OPPORTUNITIES - LOOKING AHEAD

The Photo Enforcement Program experiences both challenges and opportunities every year, including public perception; 2022 was no exception.

### **Public Perception**

As an accredited police agency with the Commission on Accreditation for Law Enforcement Agencies (CALEA), the Winnipeg Police Service is required to conduct a biennial survey of the citizens of Winnipeg to measure public opinions and perceptions of the police service. The most recent survey, conducted in 2022, showed strong support for all aspects of the Photo Enforcement Program. The survey can be found [here](#).

The approval rate for the use of photo enforcement is strong, with an average of 79% of respondents indicating strong or moderate approval.

Respondents were asked about their opinion on the use of photo enforcement at other locations. More than 80% approve of the use of PR in areas that have frequent collisions or traffic events and 65% of respondents support the use of photo enforcement in *any* location.

### **Technology**

Since the inception of the Photo Enforcement Program, the population of the City of Winnipeg has increased steadily and there are significantly more vehicles – 100,000 plus - using the roadways. The construction of new neighbourhoods and the addition of major thoroughfares, including Sterling Lyon Parkway, Chief Peguis Trail, Kenaston Boulevard South and Centreport Canada Way have impacted traffic flow and volume. However, the total number of ISC locations has remained relatively static since 2005.

The current ISC technology utilizes induction loops, which are coils of wire, embedded in the surface of the roadway to detect speed and red-light infractions. This use of induction loops makes moving the ISCs to new locations difficult and costly. In addition, this technology is at end of life and requires replacement. New technology for intersection safety cameras utilizes radar-based detection systems, which are more cost efficient for installation and adaptable for changing traffic patterns.

Only camera systems prescribed in the [Image Capturing Enforcement Regulation](#) may be used. As the approved ISCs have become obsolete, the Winnipeg Police Service made a request to the Province of Manitoba for approval of new technology, which requires an amendment to the Regulation. The approval was held in abeyance pending the completion of the previously noted provincial review of the Photo Enforcement Program announced in late 2019. As the review has been delayed indefinitely, the Service has renewed its request to authorize new camera system technology and will continue to request regulation amendments.

### Coronavirus Pandemic

There are many variables that can affect traffic patterns and collisions, including driver behavior, weather, traffic volume and enforcement. The coronavirus pandemic, which emerged in early 2020, resulted in public health restrictions that significantly impacted traffic volumes.

The following table shows monthly traffic volumes at monitored intersections from 2019 through 2022.

	Jan	Feb	Mar	Apr	May	Jun	
2019	9,376,357	9,795,140	10,633,179	8,874,786	10,542,469	9,938,162	
2020	9,447,423	9,449,211	9,061,446	6,812,393	9,401,373	10,583,138	
2021	8,581,320	8,106,123	10,214,955	10,184,308	10,740,154	10,923,129	
2022	8,158,255	7,569,219	10,348,840	9,578,595	11,208,714	10,451,684	

	Jul	Aug	Sep	Oct	Nov	Dec	Total
2019	8,610,432	10,475,500	11,359,294	12,668,486	12,319,058	10,908,681	125,501,544
2020	11,022,209	10,284,624	10,706,571	10,563,161	8,027,680	8,680,848	114,040,077
2021	11,049,525	11,393,629	10,661,007	11,236,707	10,625,255	9,735,013	123,451,125
2022	8,744,976	9,781,632	8,903,786	9,956,337	10,134,097	10,578,623	115,414,758

This data shows an overall decrease in traffic volumes from 2021, likely due to remote work, post pandemic.

### Amber-light Timings

In September 2020, the City of Winnipeg adjusted amber light durations for intersections with speed limits of 70 kph and greater using a calculation based on the posted speed limit and slope of the roadway. Previously, all amber light durations were 4-seconds. This timing adjustment increased the amber light duration at four intersections with photo enforcement equipment:

- Northbound Century at Silver (location #814)
- Westbound Bishop Grandin Blvd at River Road (location #841)
- Southbound Lagimodiere at Grassie (location #845)
- Westbound Fermor at St. Mary’s Road (location #846)<sup>1</sup>

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<sup>1</sup> the ISC location at westbound Fermor Ave at St. Mary’s Road was not active for the entire year

The following table outlines year-by-year data on the number of Red Light (RL) and Speeding (Speed on Green - SOG) violations at these intersections from 2020 through 2022.

Location Number	2020		2021		2022	
	RL Issued/Day	SOG Issued/Day	RL Issued/Day	SOG Issued/Day	RL Issued/Day	SOG Issued/Day
<b>814</b>	0.604	0.929	0.750	0.884	1.135	0.481
<b>841</b>	1.802	2.923	0.754	3.082	1.369	2.631
<b>845</b>	1.765	1.588	0.564	1.290	1.235	0.894
<b>Average</b>	1.390	1.813	0.689	1.752	1.246	1.335

The following table outlines collision information at these intersections from 2020 through 2022. Descriptions and full collision information can be found in Section 11 of this report.

Location Number	2020			2021			2022		
	Total MVC	Rear End	Rt Angle	Total MVC	Rear End	Rt Angle	Total MVC	Rear End	Rt Angle
<b>814</b>	11	4	3	11	6	2	23	12	4
<b>841</b>	47	33	1	13	22	5	50	24	5
<b>845</b>	56	35	3	20	51	1	80	47	3
<b>Average</b>	38.00	24.00	2.33	14.67	26.33	2.67	51	27.67	4

Upon reviewing data from these locations, there was an increase in the number of red-light (RL) violations per enforcement day, where the speed limit was 80 kph, but still below the number in 2020. Although the number increased for northbound Century at Silver where the speed limit is 70 kph, speeding (Speed on Green - SOG) violations decreased at all three locations.

The Winnipeg Police Service will continue to monitor these intersections to determine the impact of the change with respect to collisions.

## 5. REVENUES AND EXPENSES

All fine revenue from photo enforcement tickets is collected by the Government of Manitoba and a portion of the fine amount is remitted to the Winnipeg Police Service. For the year 2022<sup>1</sup>, the Winnipeg Police Service received revenues of \$10,925,350.

For the year 2022, the total expenses for the Photo Enforcement Program were \$5,216,807.

- the salaries and benefits of the Winnipeg Police Service members assigned to work on the Photo Enforcement program on a full-time basis (12.5%)
- the cost of working with outside contractors on enforcement (Conduent and operators) (82.1%)
- miscellaneous expenses, including training, associated with operating the program (5.4%)

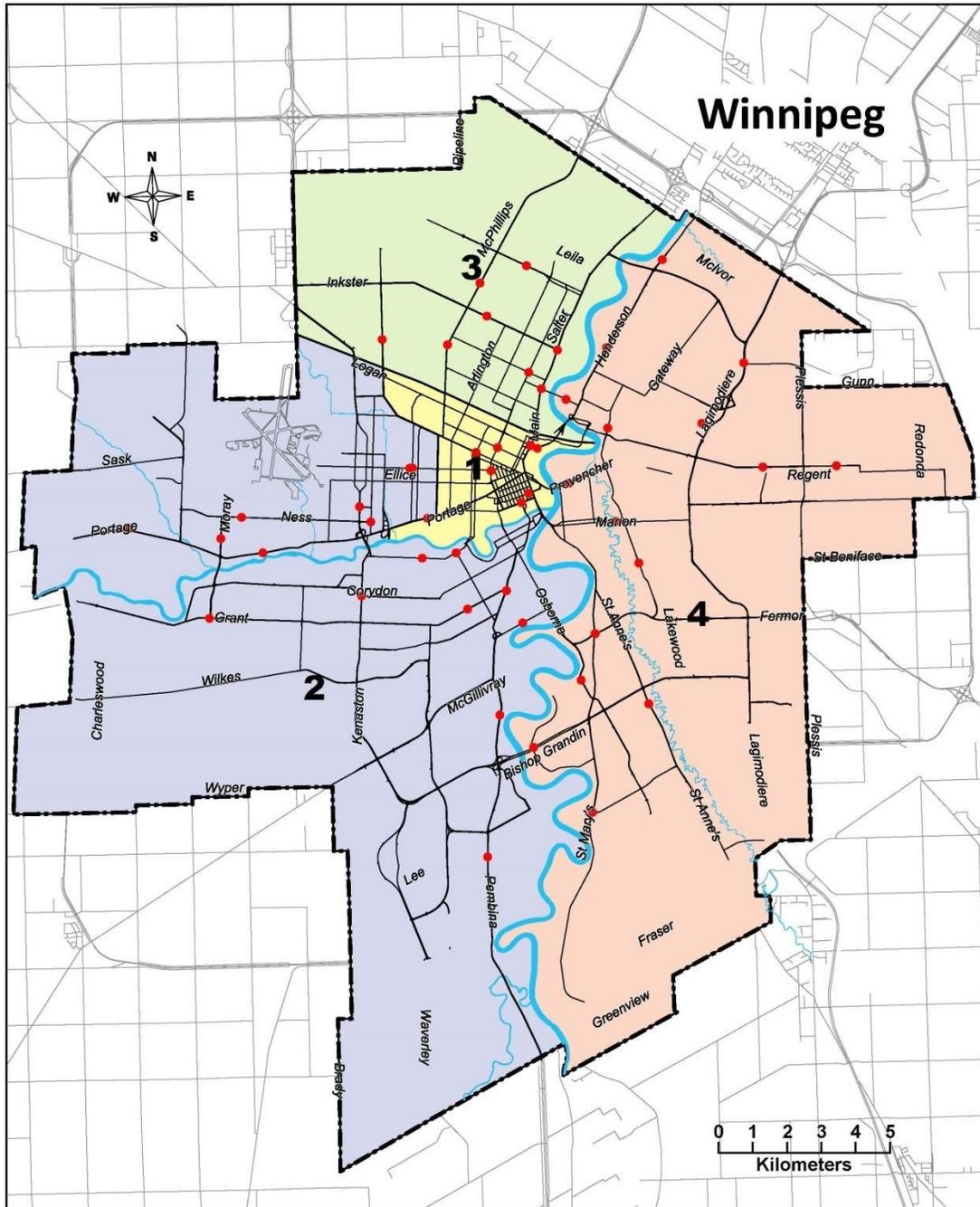
This represents a surplus of \$5,708,543. This amount is incorporated into the overall Winnipeg Police Service budget and is used for other policing purposes.

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<sup>1</sup> totals provided by Winnipeg Police Finance Division

## 6. PROGRAM OUTPUTS

### INTERSECTION SAFETY CAMERA LOCATIONS 2021



● Intersection Safety Camera Locations

**District 1**

Balmoral St. NB @ Sargent Ave.  
Disraeli Fwy. NB @ Lily St.  
Donald St. SB @ Broadway  
Isabel St. NB @ William Ave.  
Main St. NB @ Logan Ave.  
Notre Dame Ave. WB @ Sherbrook St.  
York Ave. EB @ Fort St.

**District 2**

Academy Rd. EB @ Stafford St.  
Academy Rd. WB @ Oak St.  
Century St. NB @ Silver Ave.  
Erin St. SB @ Sargent Ave.  
Grant Ave. EB @ William R. Clement Pkwy  
Grant Ave. WB @ Wilton St.  
Jubilee Ave. WB @ Cockburn St.  
Kenaston Blvd. SB @ Corydon Ave.  
Moray St. NB @ Lodge Ave.  
Ness Ave. WB @ Whytewold Rd.  
Pembina Hwy. NB @ Dalhousie Dr.  
Pembina Hwy. NB @ Grant Ave.  
Pembina Hwy. SB @ Chevrier Blvd.  
Portage Ave. EB @ Cavalier Dr.  
Portage Ave. EB @ Dominion St.  
Portage Ave. WB @ Mount Royal Rd.  
Sargent Ave. EB @ Clifton St.  
St. James St. SB @ Ness Ave.

**District 3**

Inkster Blvd. WB @ Airlies St.  
Keewatin St. NB @ Selkirk Ave.  
Leila Ave. WB @ Sinclair St.  
Main St. NB @ Inkster Blvd.  
Main St. SB @ Redwood Ave.  
McPhillips St. SB @ Jefferson Ave.  
McPhillips St. SB @ Redwood Ave.  
Salter St. SB @ Mountain Ave.

**District 4**

Archibald St. SB @ Elizabeth Rd.  
Bishop Grandin Blvd. WB @ River Rd.  
Dunkirk Dr. NB @ St. Vital Rd.  
Fermor Ave. WB @ St. Mary's Rd.  
Henderson Hwy. NB @ Gilmore Ave.  
Henderson Hwy. SB @ Kimberly Ave.  
Hespeler Ave. EB @ Beatrice St.  
Lagimodiere Blvd. SB @ Grassie Blvd.  
Marion Street EB @ Dufresne Ave.  
Panet Rd. SB @ Munroe Ave.  
Provencher Blvd. EB @ Aulneau St.  
Regent Ave. W. WB @ Owen St.  
Regent Ave. West EB @ Madeline St.  
St. Mary's Rd. NB @ Warde Ave.  
St. Anne's Rd. NB @ Meadowood Dr.  
Talbot Ave. EB @ Watt St.

**Winnipeg Police Service**



## 7. DEPLOYMENT CRITERIA

### MOBILE PHOTO RADAR UNITS

Mobile PR units are deployed between 0700 hours and 2200 hours, seven days a week throughout the City of Winnipeg. The locations where mobile units can deploy is restricted by *The Highway Traffic Act* and the *Image Capturing Enforcement Regulation*. Authorized locations are as follows:

- School zones
- Playground zones
- Construction zones

### INTERSECTION SAFETY CAMERAS

ISC are deployed throughout the City of Winnipeg and can remain in operation 24 hours a day, seven days a week. The locations where ISC may be established is restricted by *The Highway Traffic Act* and the *Image Capturing Enforcement Regulation* to intersections controlled by traffic control lights and railway crossings. The WPS does not currently have any locations established at railway crossings.

The WPS uses the following information in the decision to deploy ISC at a particular location:

- Collision data
- Speed data
- Public input
- Balanced distribution throughout the City of Winnipeg
- Technical ability to install ISC infrastructure at a particular location

## 8. PHOTO ENFORCEMENT STATISTICS

### YEAR TO YEAR DATA: 2018 – 2022<sup>2</sup>

	2018	2019	2020	2021	2022
<b>Total Number of Offences</b>					
Mobile Photo Radar (reduced speed school zone) <sup>3</sup>	41,784	41,793	33,661	45,183	44,736
Mobile Photo Radar (all other)	55,278	49,231	41,937	45,724	28,204
Intersection Safety Cameras / Speeding	34,010	28,448	26,386	27,694	27,321
Intersection Safety Cameras / Red Light	9,856	9,520	7,470	8,615	10,604
<b>Total</b>	<b>141,028</b>	<b>128,992</b>	<b>109,454</b>	<b>127,216</b>	<b>110,685</b>
<b>Clearance Status<sup>4</sup></b>					
Convictions	136,438	124,941	106,067	121,905	101,749
Total Stays - appealed or rectified	764	4	1	4	9
Total Stays - quash, acquittals, dismissed	3,100	2,863	1,890	1,571	870
Not Guilty	0	0	0	0	0
On Going - no disposition date	50	19	258	3471	7,588
On Going - previous disposition voided	1960	62	3,067	475	304
<b>Tickets Issued by Conventional Means</b>					
Speeding	13,016	14,260	13,735	12,795	7,310
Red Light	764	757	555	1,408	1,683
Stop Sign	4,630	4,513	4,122	3,873	3,629
Disobey Traffic Control Device (other)	4,567	6,970	5,486	5,361	5,044

## 9. MOBILE STATISTICS

The table below identifies the number of speeding violations captured by mobile PR in each of the four speed ranges.

	Number of Tickets Issued				Percent of Total Tickets Issued			
	PR 2019	PR 2020	PR 2021	PR 2022	PR 2019	PR 2020	PR 2021	PR 2022
<b>SPEED over limit</b>								
Subtotal 10 – 14 kph	37,145	31,536	36,187	31,295	40.81%	41.72%	39.81%	42.9%
Subtotal 15 – 19 kph	38,019	31,097	38,037	30,731	41.77%	41.13%	41.84%	42.1%
Subtotal 20 – 34 kph	15,568	12,692	16,321	10,682	17.10%	16.79%	17.95%	14.6%
Subtotal >34 kph	292	273	362	232	0.32%	0.36%	0.40%	0.50%
<b>TOTAL</b>	<b>91,024</b>	<b>75,598</b>	<b>90,907</b>	<b>72,940</b>				

<sup>2</sup> Program data provided by contract partner Conduent Business Services Canada Inc.

<sup>3</sup> Reduced zone enforcement was suspended in spring 2020 when schools were closed for remote learning

<sup>4</sup> Clearance Status and Tickets Issued by Conventional Means statistics provided by Manitoba Justice - Courts

## 10. INTERSECTION CAMERA STATISTICS

The table below identifies the number of speeding violations captured at the ISC location in each of the four speed ranges. In addition, the total number of red-light violations is noted, along with the grand totals for each of the indicated years.

	Number of tickets Issued				Percent of total tickets issued			
	ISC 2019	ISC 2020	ISC 2021	ISC 2022	ISC 2019	ISC 2020	ISC 2021	ISC 2022
<b>SPEED over limit</b>								
Subtotal 10 – 14 kph	15,509	14,065	14,127	14,250	54.52%	53.30%	51.01%	52.2%
Subtotal 15 – 19 kph	10,603	10,060	10,961	10,708	37.27%	38.13%	39.58%	39.2%
Subtotal 20 – 34 kph	2,206	2,087	2,375	2,222	7.75%	7.91%	8.58%	8.1%
Subtotal >34 kph	130	174	231	141	0.46%	0.66%	0.83%	0.5%
<b>SPEED TOTAL</b>	<b>22,448</b>	<b>26,386</b>	<b>27,694</b>	<b>27,321</b>				
<b>RED LIGHT TOTAL</b>	9,520	7,470	8,615	10,604				
<b>ISC TOTAL</b>	<b>37,968</b>	<b>33,856</b>	<b>36,309</b>	<b>37,925</b>				

The table below identifies the overall number of tickets issued per camera per day of enforcement at the start of the program compared to recent years.

ISC Speed over limit (camera/day)	Program Implementation 2003	2019	2020	2021	2022
10 – 14 kph	4.65	1.29	1.17	1.18	1.19
15 – 19 kph	6.08	0.88	0.84	0.91	0.89
20 – 34 kph	1.61	0.18	0.17	0.20	0.19
>34 kph	0.06	0.01	0.01	0.02	0.01
<b>TOTAL</b>	<b>12.40</b>	<b>2.37</b>	<b>2.20</b>	<b>2.31</b>	<b>2.28</b>

In 2003, the majority of offences at Intersection Safety Camera locations were within the range of 15 – 19 kph over the posted speed limit. Presently, the majority are in the range 10 – 14 kph over the posted speed limit. At program implementation, the total number of offences captured per ISC per day was in excess of 12. In 2022, that number has been reduced to 2.28 offences per camera per day of operation. This shows not only a reduction in the overall number of speeding violations, but an overall shift to reduce speeds in these areas.

The table below indicates the number of tickets issued by location for the original 12 and remaining 37 active ISC locations. Of these, two denote location code changes through street names or direction. The significant reductions since the program’s beginning indicate the program is effectively reducing red light and speeding offences in these areas.

Tickets Issued by ISC - ALL						TOTAL (since installation)
District	Location Description	2019	2020	2021	2022	
<b>First 12</b>						
1	Sherbrook St. NB @ Broadway <sup>[1]</sup>	0	0	0	0	0
4	Provencher Blvd. EB @ Aulneau St.	0	0	0	0	14,231
4	Marion Street EB @ Dufresne Ave	526	419	473	2	16,769
4	Talbot Ave. EB @ Watt St.	80	224	269	207	3,522
4	Hespeler Ave. EB @ Beatrice St.	1699	1676	1597	1250	35,944
4	Henderson Hwy. SB @ Kimberly Ave.	588	409	0	0	11,610
3	Inkster Blvd. WB @ Airlies St.	475	385	743	117	23,726
3	Salter St. SB @ Mountain Ave.	0	0	0	0	4,284
1	Isabel St. NB @ William Ave.	0	0	0	0	14,291
2	Jubilee Ave. WB @ Cockburn St	0	0	0	0	3,529
2	Academy Rd. WB @ Oak St.	589	248	813	739	17,075
2	Moray St. NB @ Lodge Ave.	430	533	625	767	17,174
<b>Second 12</b>						
2	Ness Ave. WB @ Whytefold Rd.	212	125	403	4	4,435
2	Century St. NB @ Silver Ave.	167	500	562	564	8,097
1	Sargent Ave. EB @ Clifton St.	527	318	319	292	21,419
3	Keewatin St. NB @ Selkirk Ave.	179	0	0	413	11,696
3	Leila Ave. WB @ Sinclair St.	268	43	0	0	4,102
2	Kenaston Blvd. SB @ Corydon Ave.	2414	3431	3058	2619	80,261
1	Donald St. SB @ Broadway	479	750	93	725	17,051
4	Archibald St. SB @ Elizabeth Rd.	0	0	0	0	9,948
4	Regent Ave. W. WB @ Owen St.	1842	2000	1537	2661	36,350
4	St. Mary's Rd. SB @ Warde Ave. <sup>[2]</sup>	0	0	0	0	18,283
2	Pembina Hwy. NB @ Dalhousie Blvd. <sup>[3]</sup>	0	0	0	0	19,924
2	Portage Ave. EB @ Cavalier Dr.	1175	1144	1120	601	25,908
<b>Third 12</b>						
3	Main St. SB @ Redwood Ave.	476	339	701	556	6,475
3	McPhillips St. SB @ Redwood Ave.	537	449	548	566	7,475
1	Notre Dame Ave. WB @ Sherbrook St.	0	0	0	0	5,000
2	Grant Ave. WB @ Wilton St.	2116	736	2019	1125	70,238
2	Ellice Ave. WB @ St. James St. <sup>[4]</sup>	0	0	0	0	18,080
4	Panet Rd. SB @ Munroe Ave.	131	329	1140	1040	14,931
2	Portage Ave. WB @ Mount Royal Rd.	411	0	0	0	23,664
1	Disraeli Fwy. NB @ Lily St.	1139	1227	1200	1232	28,050
4	St. Anne's Rd. NB @ Meadowood Dr.	72	174	279	343	7,851
4	Dunkirk Dr. NB @ St. Vital Rd.	474	427	171	185	6,079
4	Regent Ave. West EB @ Madeline St.	1195	1163	1098	809	32,455
2	Pembina Hwy. NB @ Grant Ave.	1511	1421	1667	1619	25,296

Tickets Issued by ISC - ALL						TOTAL
District	Location Description	2019	2020	2021	2022	(since installation)
<b>Additional Locations</b>						
1	Main St. NB @ Logan Ave.	4454	3756	4276	3069	91,238
2	Portage Ave. EB @ Dominion St.	997	778	944	1031	9,577
3	Main St. NB @ Inkster Blvd.	689	604	698	571	10,399
4	Henderson Hwy. NB @ Gilmore Ave.	398	376	426	337	6,846
4	Bishop Grandin Blvd. WB @ River Rd.	2404	1602	1354	1432	45,879
2	Grant Ave. EB @ Charleswood Parkway <sup>[5]</sup>	0	0	0	0	4,540
2	Kenaston Blvd. NB @ McGillivray Blvd	0	0	0	0	885
1	York Ave. EB @ Fort St.	1193	744	1264	802	13,025
4	Lagimodiere Blvd. SB @ Grassie Blvd.	1698	1214	569	762	17,169
2	Fermor Ave. WB @ St. Mary's Rd.	0	1	12	8	453
2	St. James St. SB @ Ness Ave.	54	246	238	260	2,275
3	McPhillips St. SB @ Jefferson Ave.	49	430	501	437	7,437
2	St Mary's Rd. NB @ Warde Ave. <sup>[2]</sup>	1087	907	957	875	14,450
1	Balmoral St. NB @ Sargent Ave.	844	897	874	824	8,300
2	Pembina Hwy. SB @ Chevrier Blvd.	1227	1342	1505	647	13,719
2	Erin St. SB @ Sargent Ave.	1	0	0	0	448
2	Grant Ave. EB @ William R. Clement Pkwy <sup>[5]</sup>	402	396	421	507	5,207
2	Academy Rd. EB @ Stafford St.	0	0	0	0	4,198
2	Pembina Hwy. NB @ Bairdmore Blvd <sup>[3]</sup>	2759	2093	1835	1694	18,204
5	Marion Street EB @ Dufresne Ave <sup>[7]</sup>				6224	6,224
5	Fermor Ave. WB @ St. Mary's Rd. <sup>[6]</sup>				9	9

[1] Inactive (Removed)

[2] Direction change from S/B to N/B November 2009

[3] Cross street name changed from Dalhousie Blvd. to Bairdmore Blvd June 2015

[4] Inactive (Removed)

[5] Street name changed from Charleswood Parkway to William R Clement Parkway June 2010

[6] Speed reduced from 70 km/hr to 60 km/hr – August 29, 2022

[7] Speed reduced from 60 km/hr to 50 km/hr – February 10, 2022

## 11. TRAFFIC COLLISION STATISTICS

### 2022 COLLISION STATISTICS SUMMARY

This report summarizes 2022 collision statistics for the intersections that have Intersection Safety Cameras (ISCs).

In October 2011, amendments to *The Highway Traffic Act* created two sources for Traffic Accident Reports (TARs); TARs completed by law enforcement agencies and TARs completed when a collision claim is reported to Manitoba Public Insurance. This change has helped correct the underreporting of collisions that resulted in minimal injury or property damage that occurred prior to the amendments. In this report, TAR information has been merged with collision claims information to match collision counts and collision configuration with intersection data. This information is aggregated to the incident level.

This report includes all intersections that have, or have had, an ISC. *Due to the potential for revisions to collision claims information and the nature of sourcing data from a live database, the 2022 collision totals are subject to change over time.*

#### Definitions:

**MVC Injury:** Is a traffic collision where a person has been killed or injured. Due to the small number of fatal collisions, fatal and injury collisions have been combined and denoted as injury.

**MVC PDO:** Is a traffic collision where there is physical damage to property/vehicles, but with no people killed or injured.

**MVC Total:** Injury and PDO collisions are combined in an overall collision summary.

**Rear End:** Is any traffic collision where one vehicle collides with the rear section of another vehicle that is travelling in the same direction.

**Rear End Injury:** Is a traffic collision where one vehicle collides with the rear section of another vehicle that is travelling in the same direction and results in at least one injury or fatality.

**Rt Angle:** Is any 90-degree traffic collision at an intersection where one vehicle collides with the side of another vehicle. It is important to note that right angle collisions are synonymous with a minimum of one or two vehicles entering an intersection when it is either unsafe to do so, or in direct contravention of a red traffic signal.

**Rt Angle Injury:** Is a 90-degree traffic collision at an intersection where one vehicle collides with the side of another vehicle and results in at least one injury or fatality.

**L/Rt Turn:** Is any traffic collision at an intersection where one vehicle collides with another vehicle while conducting either a left or right turn. This collision could be with vehicles across from each other, travelling in opposing directions, or travelling in the same direction. The left and right turn collisions exclude right angle (Rt Angle) collisions.

**L/Rt Turn Injury:** Is a traffic collision at an intersection where one vehicle collides with another vehicle while conducting either a left or right turn and results in at least one injury or fatality. This collision could be with vehicles across from each other, travelling in opposing directions, or

travelling in the same direction. The left and right turn injury collisions exclude right angle (Rt Angle) injury collisions.

## Traffic Collision Statistics 2022 <sup>5</sup>

The following table outlines the 2022 collision statistics summary for Winnipeg intersections that have Intersection Safety Cameras (ISCs).

- The 2022 collision counts at ISC Intersections are preliminary and subject to change.
- Injury collisions are included in the total collision counts.
- Injury collisions include fatal and injury crashes.
- PDO collisions exclude all injury collisions as defined.
- Non-applicable categories not shown.

Street 1	Street 2	MVC Injury	MVC PDO	MVC Total	Rear End	Rear End Injury	Rt Angle	Rt Angle Injury	L/Rt Turn	L/Rt Turn Injury
Academy Rd	Oak St	4	1	5	4	4	0	0	0	0
Academy Rd	Stafford St	14	30	44	32	10	3	1	0	0
Airlies St	Inkster Blvd	1	3	4	3	1	0	0	0	0
Archibald St	Elizabeth Rd	4	13	17	4	0	0	0	2	0
Aulneau St	Provencher Blvd	2	3	5	4	2	0	0	0	0
Balmoral St	Sargent Ave	5	16	21	7	1	8	1	0	0
Beatrice St	Hespeler Ave	1	7	8	3	1	3	0	0	0
Bishop Grandin Blvd	River Rd	6	44	50	24	2	5	1	2	1
Broadway	Donald St	14	31	45	9	3	20	9	5	2
Broadway	Sherbrook St	5	21	26	8	1	6	1	1	0
Cavalier Dr	Portage Ave	11	44	55	12	5	1	1	6	5
Century St	Silver Ave	5	18	23	12	1	4	3	1	0
Chevrier Blvd	Pembina Hwy	3	32	35	12	2	5	0	2	0
Clifton St	Sargent Ave	3	1	4	0	0	2	2	0	0
Cockburn St S	Jubilee Ave	0	5	5	1	0	0	0	0	0
Corydon Ave	Kenaston Blvd	15	43	58	34	12	0	0	4	1
Dalhousie Dr	Pembina Hwy	10	51	61	25	5	9	2	2	1
Disraeli Fwy	Lily St	1	5	6	4	1	1	0	0	0
Dominion St	Portage Ave	5	15	20	9	2	2	0	2	2
Dufresne Ave	Marion St	2	3	5	4	2	0	0	0	0
Dunkirk Dr	St Vital Rd	1	0	1	1	1	0	0	0	0

<sup>5</sup> Collision Statistics provided by MPI

Street 1	Street 2	MVC Injury	MVC PDO	MVC Total	Rear End	Rear End Injury	Rt Angle	Rt Angle Injury	L/Rt Turn	L/Rt Turn Injury
Ellice Ave	St James St	10	34	44	14	6	5	3	0	0
Erin St	Sargent Ave	6	17	23	5	1	8	4	0	0
Fermor Ave	St Mary's Rd	12	47	59	32	7	2	1	1	1
Fort St	York Ave	2	6	8	0	0	5	2	0	0
Gilmore Ave	Henderson Hwy	4	5	9	6	4	0	0	0	0
Grant Ave	Pembina Hwy	7	40	47	13	4	3	1	2	1
Grant Ave	William R. Clement Pkwy	3	3	6	3	2	0	0	1	1
Grant Ave	Wilton St	5	8	13	3	3	3	1	0	0
Grassie Blvd	Lagimodiere Blvd	23	57	80	47	15	3	2	2	1
Henderson Hwy	Kimberly Ave	7	16	23	8	1	4	2	1	0
Inkster Blvd	Main St	9	31	40	13	6	3	0	1	1
Isabel St	William Ave	6	13	19	8	1	3	2	0	0
Jefferson Ave	McPhillips St	23	67	90	33	8	9	4	7	2
Keewatin St	Selkirk Ave	6	17	23	8	2	4	2	0	0
Kenaston Blvd	McGillivray Blvd	30	115	145	58	19	11	5	7	0
Leila Ave	Sinclair St	4	18	22	8	3	2	0	0	0
Lodge Ave	Moray St	0	4	4	1	0	1	0	0	0
Logan Ave	Main St	6	21	27	9	3	6	2	1	0
Madeline St	Regent Ave W	3	4	7	2	2	0	0	2	1
Main St	Redwood Ave	15	51	66	25	8	6	3	4	1
McPhillips St	Redwood Ave	3	11	14	4	1	4	2	0	0
Meadowood Dr	St Anne's Rd	4	11	15	7	2	3	1	1	1
Mount Royal Rd	Portage Ave	5	14	19	9	3	2	0	0	0
Mountain Ave	Salter St	8	14	22	1	1	6	3	1	1
Munroe Ave	Panet Rd	6	12	18	12	5	1	0	0	0
Ness Ave	St James St	11	25	36	16	6	4	2	1	0
Ness Ave	Whytefold Rd	9	19	28	7	3	4	2	3	0
Notre Dame Ave	Sherbrook St	7	23	30	15	5	7	2	1	0
Owen St	Regent Ave W	10	10	20	7	4	3	2	6	2
St Mary's Rd	Warde Ave	1	13	14	3	0	2	1	1	0
Talbot Ave	Watt St	5	31	36	16	2	4	0	5	2
<b>TOTAL</b>		<b>362</b>	<b>1143</b>	<b>1505</b>	<b>605</b>	<b>183</b>	<b>187</b>	<b>70</b>	<b>75</b>	<b>27</b>

Please note:

- Dalhousie Dr at Pembina Hwy includes Bairdmore Blvd at Pembina Hwy
- Mount Royal Cres at Portage Ave includes Mount Royal Rd at Portage Ave



## ANALYSIS VERSUS PROGRAM START <sup>7</sup>

On January 7, 2003, the Winnipeg Police Service began issuing tickets for speeding and red-light offences at the 12 ISC locations that were being tested in the fall of 2002. Current year comparative analysis was conducted for the original 12 ISC locations, and normalized to account for population and registered motor vehicle growth in the Winnipeg region.

**Active Major Use 1 Vehicles as at March 1 of Each Year**  
(data as of April 1, 2023)

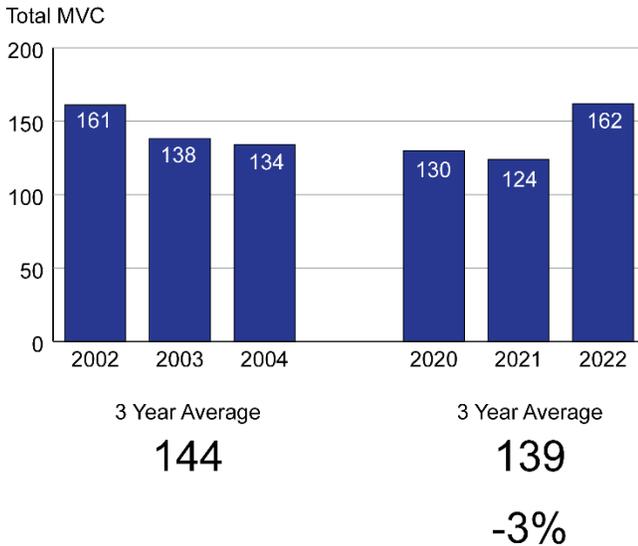
Year	Vehicle Count
2003	351,498
2004	357,594
2017	439,805
2018	446,685
2019	451,345
2020	455,499
2021	457,701
2022	463,652

### Motor Vehicle Collisions

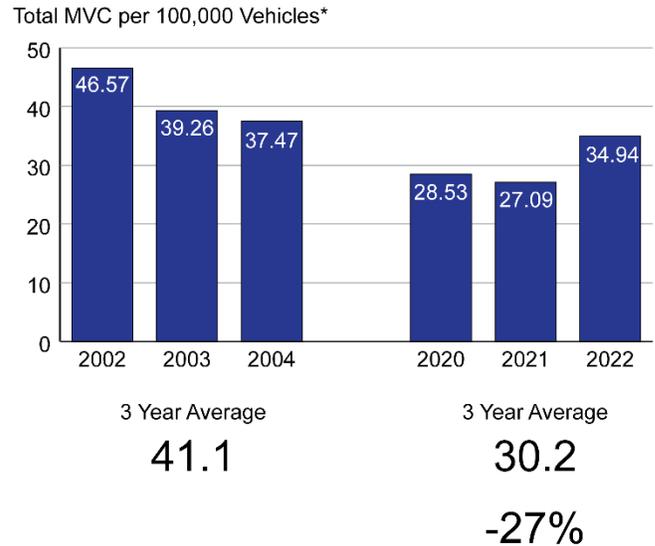
Compared to 2002, there was a 1% increase in total motor vehicle collisions (MVCs) in the 12 original ISC locations (161 vs. 162), and when normalized to account for vehicle growth there has been a 25% decrease (46.6 MVC per 100,000 vehicles vs. 34.94 MVC per 100,000 vehicles). Three-year averages between the two study periods also show a 27% decrease per capita in MVCs.

### Original 12 ISC Locations

Total Motor Vehicle Collisions (MVC) vs. Program Start  
**2022 vs. 2002: +1%**



Motor Vehicle Collisions (MVC) Normalized for Regional Motor Vehicle Growth: **2022 vs. 2002: -25%**



\*Active Major Use 1 Vehicles as of March 1 of each year: Territory 1 and commuter vehicles from Territory 2 (represents registered vehicles and commuters for Winnipeg MPI territory)  
- data source MPI, data as of Apr 1, 2023

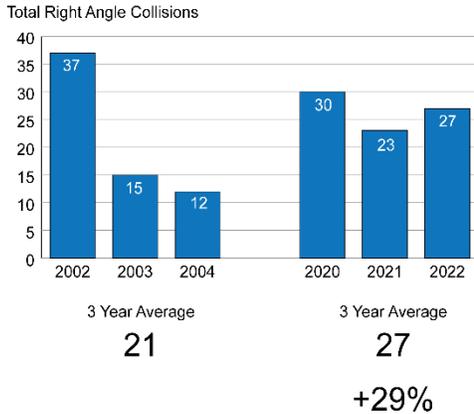
<sup>7</sup> Data analysis provided by the WPS Organizational Development & Support Division

### Right Angle Collisions (RACs)

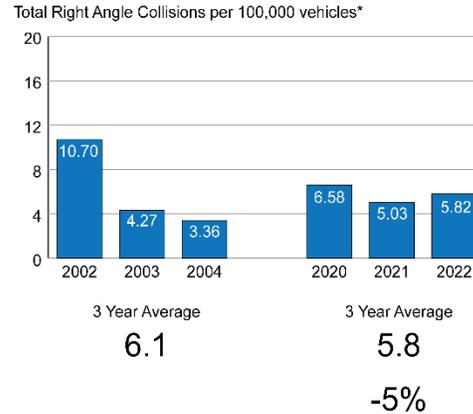
Compared to 2002, there was a 27% decrease in total right-angle collisions in the 12 original ISC locations (37 vs. 27), and when normalized to account for vehicle growth there has been a decrease of 46% (10.7 RACs per 100,000 vehicles vs. 5.82 RAC per 100,000 vehicles). Three-year averages between the two study periods show a 5% per capita decrease in right angle collisions. The initial rollout year of 2003 had the largest decrease in right angle collisions (-22, -168%).

#### Original 12 ISC Locations

Total Right Angle Collisions vs. Program Start  
2022 vs. 2002: **-27%**



Right Angle Collisions Normalized for Regional Motor Vehicle Growth: 2022 vs. 2002: **-46%**



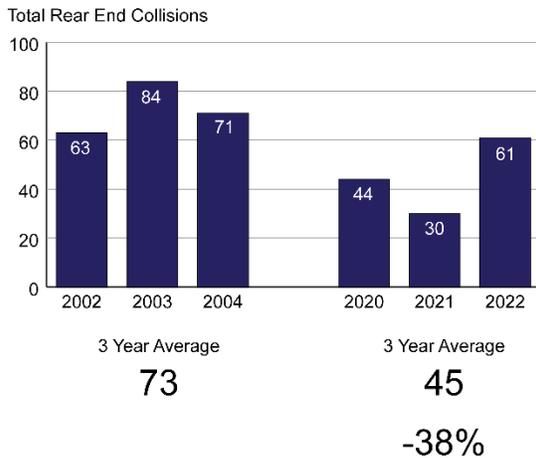
\*Active Major Use 1 Vehicles as of March 1 of each year; Territory 1 and commuter vehicles from Territory 2 (represents registered vehicles and commuters for Winnipeg MPI territory) - data source MPI, data as of Apr 1 2023

### Rear End Collisions (RECs)

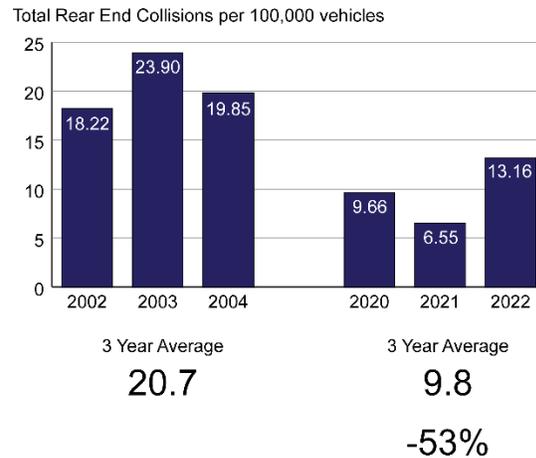
Compared to 2002, there was a 3% decrease in total rear end collisions in the 12 original ISC locations (63 vs. 61). When normalized to account for vehicle growth there has been a decrease of 28% (18.2 RECs per 100,000 vehicles vs. 13.16 REC per 100,000 vehicles). The current 3-year average is 53% lower in per capita collisions than the rollout period.

#### Original 12 ISC Locations

Total Rear End Collisions vs. Program Start  
2022 vs. 2002: **-3%**



Rear End Collisions Normalized for Regional Motor Vehicle Growth: 2022 vs. 2002: **-28%**

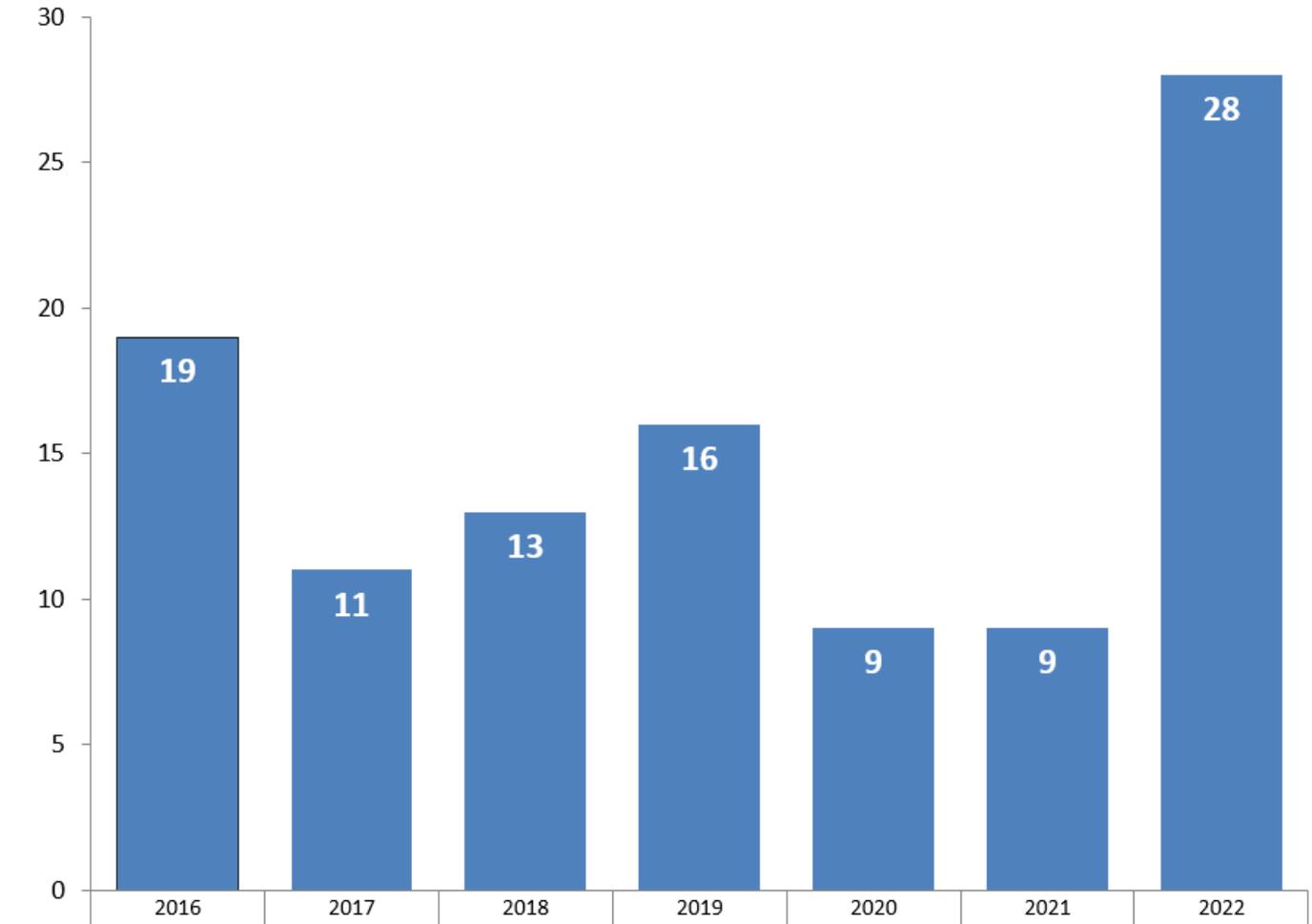


\*Active Major Use 1 Vehicles as of March 1 of each year; Territory 1 and commuter vehicles from Territory 2 (represents registered vehicles and commuters for Winnipeg MPI territory) - data source MPI, data as of April 1, 2023

**TOTAL NUMBER OF FATAL COLLISIONS (CITY OF WINNIPEG)<sup>8</sup>**

The following chart depicts the number of fatal collisions in the City of Winnipeg. There have been no recorded fatalities for 2022 at any of the intersections monitored by intersection safety cameras.

**Fatal Collisions**



<sup>8</sup> Total number of fatal collisions for 2020 corrected from 2020 Annual Report.

## 12. MOBILE PHOTO RADAR ANALYSIS

The City of Winnipeg began the Photo Enforcement Program with a 'warning' phase in November 2002. A component of the program was the deployment of mobile PR units to school and playground zones. The speed of each vehicle that passed by a deployed unit was captured electronically and stored on digital memory. This data collection feature provided insights into traffic flows in the areas monitored.

The reported number of speeding violations issued by the mobile PR units in 2003 was 103,093. For 2005, the issued violations were 48,194 for an overall decrease of 53% in comparison to 2003.

For 2007, although the program doubled its enforcement capabilities in June of 2006, the number of violations issued (74,442) represented an overall decrease of 27.8% when compared to 2003.

In 2022, mobile PR unit deployments captured 72,940 violations which represent a decrease of 19.76% compared to 2021. Compared to 2003, there was a decrease of 29.25% violations despite an increase in enforcement capability and the implementation of Reduced Speed School Zones in late 2014.

The following table represents the total number of tickets issued by speed range for 2017 through 2022 at mobile PR locations. Information from 2003 provided as a baseline.

Number of tickets Issued							
	PR	PR	PR	PR	PR	PR	PR
SPEED over limit	*2003	2017	2018	2019	2020	2021	2022
Subtotal 10 – 14 kph	49,097	38,678	41,535	37,145	31,536	36,187	31,295
Subtotal 15 – 19 kph	42,094	40,823	40,051	38,019	31,097	38,037	30,731
Subtotal 20 – 34 kph	11,526	17,537	15,358	15,568	12,692	16,321	10,682
Subtotal >34 kph	376	283	218	292	273	362	232
<b>TOTAL</b>	<b>103,093</b>	<b>97,321</b>	<b>97,162</b>	<b>91,024</b>	<b>75,598</b>	<b>90,907</b>	<b>72,940</b>
<b>Violations per 100,000 vehicles</b>	<b>29,330</b>	<b>22,128</b>	<b>21,752</b>	<b>20,167</b>	<b>16,597</b>	<b>19,862</b>	<b>15,732</b>

The number of violations per 100,000 registered vehicles decreased significantly from 19,862 in 2021 to 15,732 in 2022, a decrease of 20.79%.

Percent of total tickets issued							
	PR	PR	PR	PR	PR	PR	PR
SPEED over limit	*2003	2017	2018	2019	2020	2021	2022
Subtotal 10 – 14 kph	47.60%	39.74%	42.75%	40.81%	41.72%	39.81%	42.9%
Subtotal 15 – 19 kph	40.83%	41.95%	41.22%	41.77%	41.13%	41.84%	42.1%
Subtotal 20 – 34 kph	11.18%	18.02%	15.81%	17.10%	16.79%	17.95%	14.6%
Subtotal >34 kph	0.36%	0.29%	0.22%	0.32%	0.36%	0.40%	0.3%

## 13. PUBLIC EDUCATION CAMPAIGN

### **Just Slow Down**

Over the past 14 years, the Winnipeg Police Service has been dedicated to supporting a public safety message specifically related to speed reduction. The campaign is titled “Just Slow Down”.

### **Just Slow Down Campaign Objectives**

- To encourage Winnipeg drivers to reduce speed
- To build awareness and understanding of the relationship between speed and collisions/fatalities/serious injury
- To build general support for photo enforcement technologies as an effective tool to reduce driver speed on Winnipeg streets
- To build support for increased traffic unit flexibility to move mobile photo radar units to areas where they will be most effective
- To proactively promote safe Winnipeg streets

### **Target Audience**

- TikTok targeted the 18-24 age group
- Meta (Facebook/Instagram) targeted the 25-64 age group
- By using both platforms, a broad audience of drivers 16+ was reached.

In 2019, following a Request for Proposals, marketing consultant Argyle was selected and tasked with the development and release of a media strategy in line with the goals and objectives of the campaign.

Campaigns for 2022 built on objectives and messaging from the previous two years to increase awareness, reach the public and educate people on the benefits of photo enforcement. These messages, highlighting the decrease in red light tickets, were launched on television, radio and billboards throughout the city. Digital ads appeared on social media platforms as well as on a variety of media-related websites, directing viewers to the Just Slow Down website (<https://justslowdown.ca/>).

By far the most successful advertising platform used was Facebook/Instagram. In 2022, the digital campaign delivered 2.2 million impressions and 912,167 video views. Video watch time increased across TikTok and Meta from the prior campaign, indicating effective strategy between targeting, messaging and creative assets. Of note, 95 percent of visitors to the Just Slow Down website were new users.

2022

## Annual Report

**Winnipeg Police Service**

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**[www.winnipeg.ca/police](http://www.winnipeg.ca/police)**

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